

NOTES:

1. WEAKENED PLANE JOINTS SHALL BE USED AT REGULAR INTERVALS 10 FEET IN WALKS AND 20 FEET IN CURB AND GUTTERS
2. 1/4" EXPANSION JOINT FELT SHALL BE PLACED AT 60' INTERVALS IN CURB, GUTTER AND WALK, AT THE BCR AND ECR, AT DRIVEWAYS, AND AROUND UTILITY POLES IN SIDEWALK AREAS. SIDEWALK AND CURB JOINTS SHALL BE ALIGNED.
3. ALL CONCRETE WORK SHALL BE ONE COURSE. NO TOPPING PERMITTED.
4. ALL CONCRETE WORK SHALL BE ONE COURSE. NO TOPPING PERMITTED.
5. MATCH JOINT LINES WHEN REPLACING EXISTING SIDEWALK.
6. CURB FACE SHALL BE A STEEL TROWEL AND FINE HAIR BROOM FINISH.
7. FLOWLINE OF THE GUTTER SHALL HAVE A 4" SHINER.
8. ALL GUTTERS SHALL BE 2' UNLESS APPROVED BY CITY ENGINEER
9. REMOVE A 2' WIDE BY 14" SECTION OF PAVEMENT ALONG THE EDGE OF THE EXISTING GUTTER. RE-COMPACT BASE OR SUB-GRADE AND CONSTRUCT 8" AC OVER 6" CMB, ASPHALT SHALL BE FLUSH WITH GUTTER.

STANDARD SIDEWALK, CURB AND GUTTER  
 CITY OF VERNON, CALIFORNIA  
 COMMUNITY SERVICES DEPARTMENT  
 FEBRUARY, 2016

18+1	COUNTY	ROUTE	POST MILES	TOTAL SHEETS

**H. Paul Cohen**  
REGISTERED CIVIL ENGINEER

March 21, 2014  
ILLINOIS EXPIRES DATE

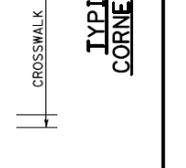
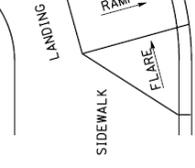
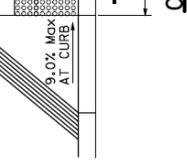
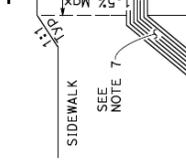
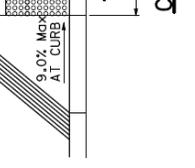
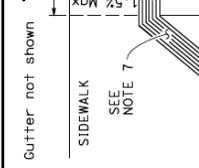
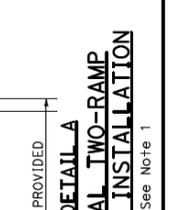
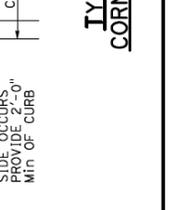
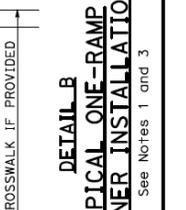
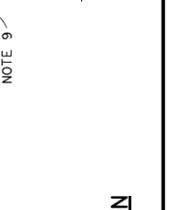
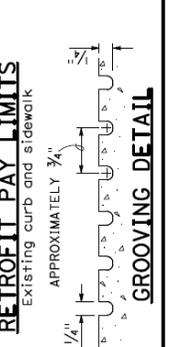
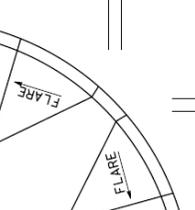
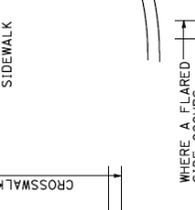
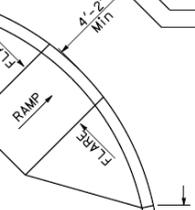
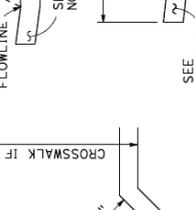
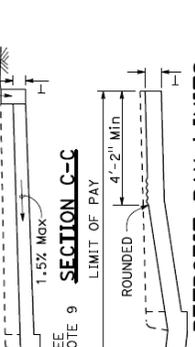
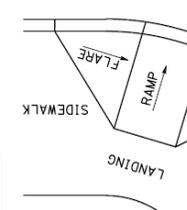
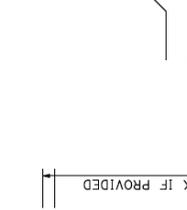
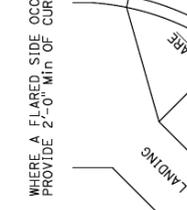
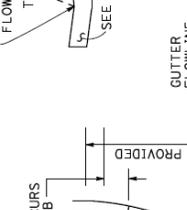
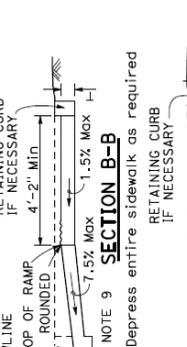
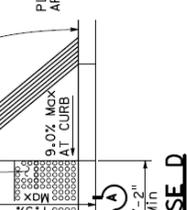
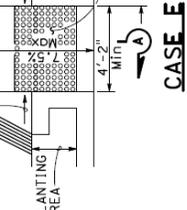
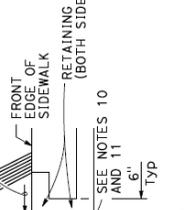
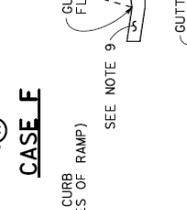
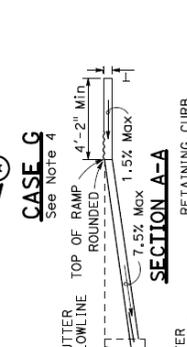
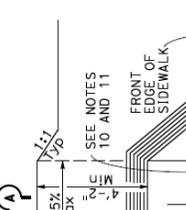
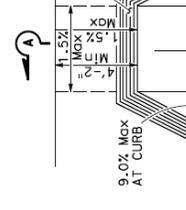
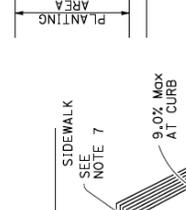
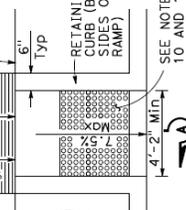
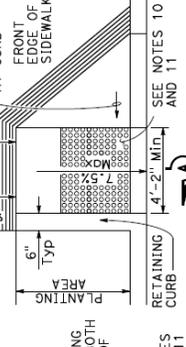
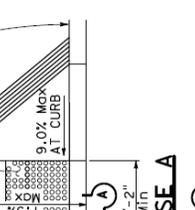
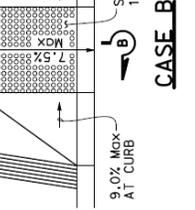
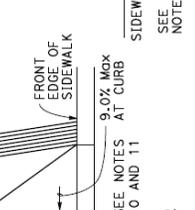
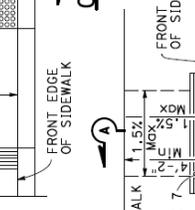
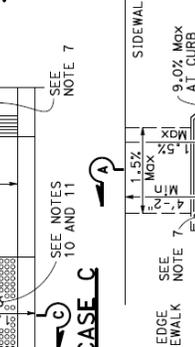
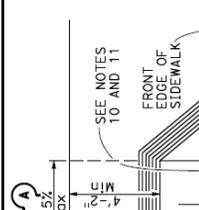
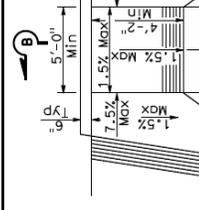
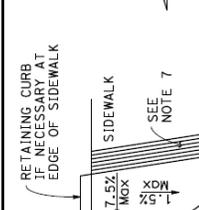
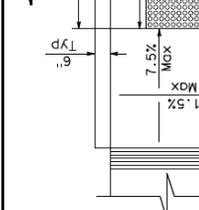
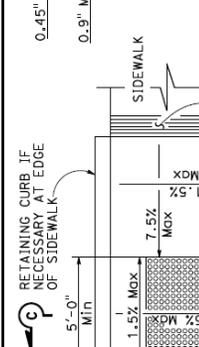
NO STATE OF CALIFORNIA FOR ITS OFFICERS  
AN AGENT SHALL NOT BE RESPONSIBLE FOR  
CONSEQUENCES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER  
Hector David Cardona  
No. 041957  
Exp. 3-31-14  
STATE OF CALIFORNIA

**RAISED TRUNCATED DOME**

TO ACCOMPANY PLANS DATED \_\_\_\_\_

- NOTES:**
- As site conditions dictate, Case A through Case G curb ramps may be used for corner installations similar to those shown in Detail A and Detail B. The case of curb ramps used in Detail A do not have to be the same. Case A through Case G curb ramps also may be used at mid block locations, as site conditions dictate.
  - If distance from curb to back of sidewalk is too short to accommodate ramp and 4'-2" platform (landing) as shown in Case A, the sidewalk may be depressed longitudinally as in Case B, or C or may be widened as in Case D.
  - When ramp is located in center of curb return, crosswalk configuration must be similar to that shown for Detail B.
  - As site conditions dictate, the retaining curb side and the flared side of the Case G ramp shall be constructed in reversed position.
  - If located on a curve, the sides of the ramp need not be parallel, but the minimum width of the ramp shall be 4'-2".
  - Side slope of ramp flares vary uniformly from a maximum of 9.0% at the curb to 1.5% at the back of sidewalk.
  - The curb ramp shall be outlined, as shown, with a 1'-0" wide border with 1/4" grooves approximately 3/4" on center. See grooving detail.
  - Transitions from ramps and landing to walks, gutters or streets shall be flush (no lip) and free of abrupt changes.
  - Counter slopes of adjoining gutters and road surfaces immediately adjacent to and within 24 inches of the curb ramp shall not be steeper than 1:20 (5.0%). Gutter pan slope shall not exceed 1" of depth for each 2'-0" of width.
  - Curb ramps shall have a detectable warning surface that extends the full width and 3'-0" depth of the ramp. A 4'-0" wide detectable warning surface may be used on a 4'-2" wide curb ramp. Detectable warning surfaces shall conform to the requirements in the Standard Specifications.
  - The edge of the detectable warning surface nearest the street shall be between 6" and 8" from the gutter flowline.
  - Sidewalk and ramp thickness, "T", shall be 3/2" minimum.
  - Utility pull boxes, manholes, vaults and all other utility facilities within the boundaries of the curb ramp will be relocated or adjusted to grade by the owner prior to, or in conjunction with, curb ramp construction.
  - Detectable warning surface may have to be cut to allow removal of utility covers while maintaining full detectable warning width and depth.



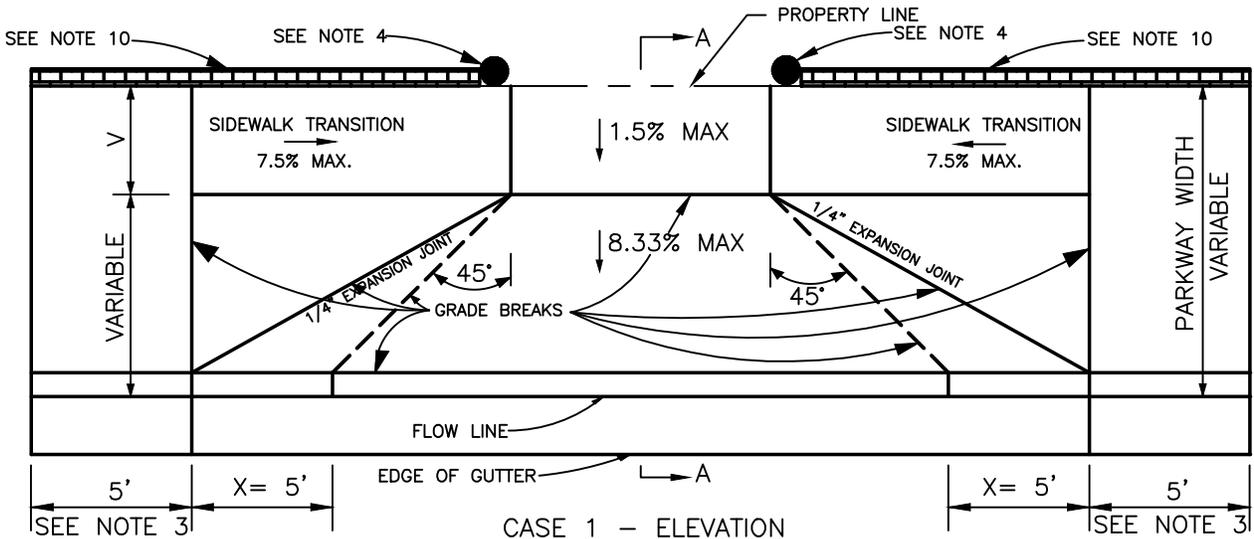
Gutter not shown

SEE NOTES 10 AND 11

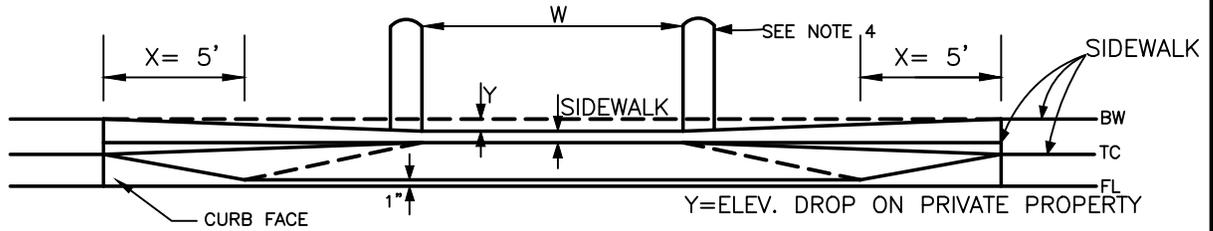
SEE NOTE 7

CASE 1 - PLAN VIEW

SEE PARKWAY WIDTH TABLE FOR APPROPRIATE CASE

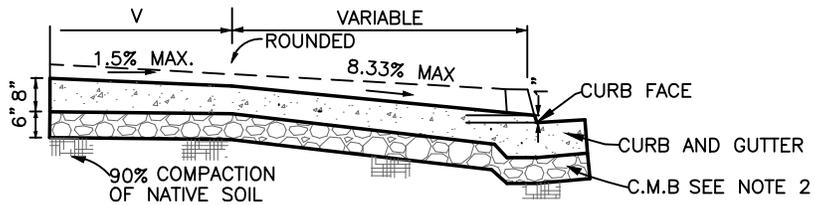


CASE 1 - ELEVATION



NOT TO SCALE

CASE 1 - SECTION AA



PARKWAY WIDTH TABLE

PARKWAY WIDTH	Y			
	6" CURB	8" CURB	10" CURB	
LESS THAN 8'	REFER TO CASE 2			
V=4'	8'	2.2"	4.2"	6.2"
	9'	1.4"	3.4"	5.4"
	10'	0.7"	2.7"	4.7"
	11'	0.0	1.9"	3.9"
	12'	0.0	1.2"	3.2"

\* REFER TO CASE 2

REVISIONS		
DATE	DESCRIPTION	INITIALS
4/3/03	CREATED	LM
12/05	CREATED ANGLED APPROACH DRIVEWAY	EM
8/07	MODIFIED CASE #2 DRIVEWAY	PC
8/15	MODIFIED CASE #1 & CASE #2 DRIVEWAY	BM
2/16	MODIFIED CASE #1 & CASE #2 DRIVEWAY	FS

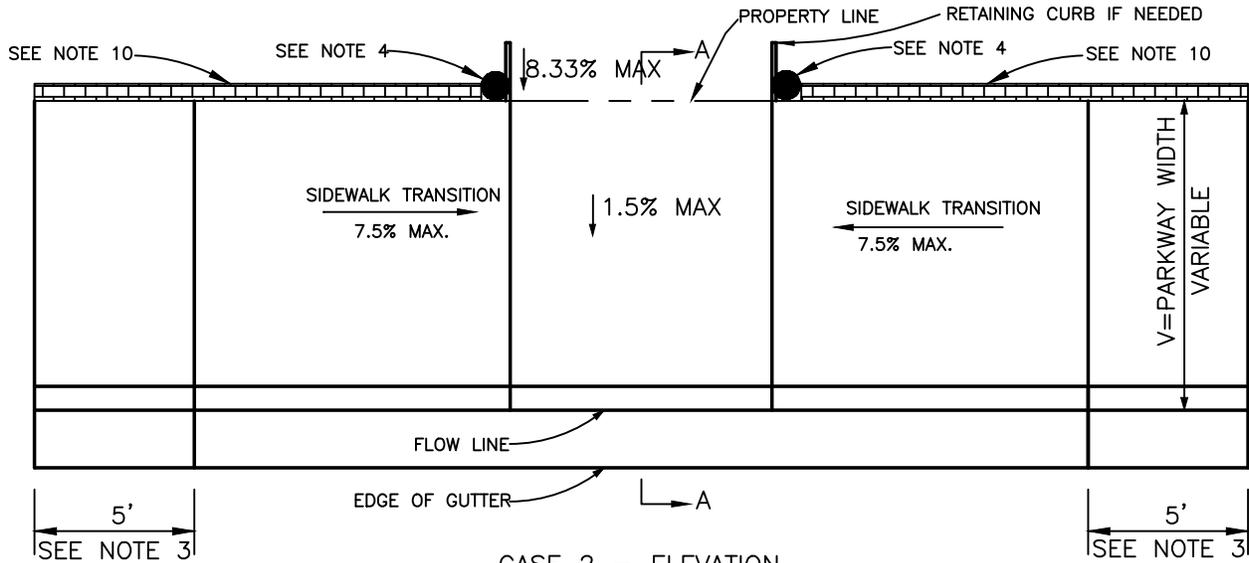
STANDARD DRIVEWAY

CITY OF VERNON, CALIFORNIA  
PUBLICWORKS, WATER & DEVELOPMENT SERVICES  
FEBRUARY 2016

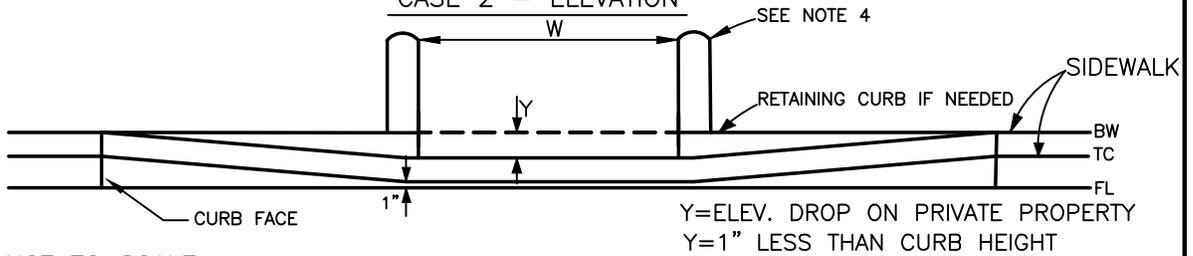
STANDARD PLAN  
PV 693  
PAGE 1 OF 2

CASE 2 - PLAN VIEW

SEE THE TABLE ON PAGE 1 OF 2 FOR APPROPRIATE CASE

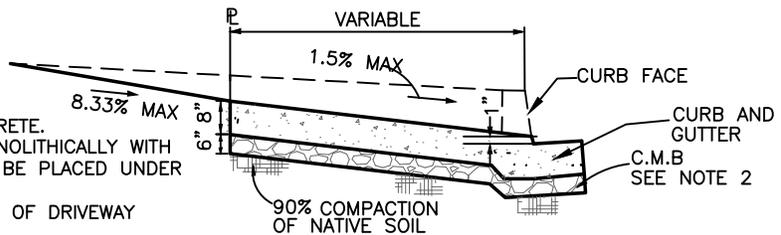


CASE 2 - ELEVATION



NOT TO SCALE

CASE 2 - SECTION AA



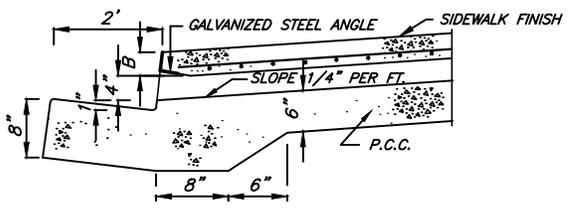
NOTES:

1. USE CLASS 520-C-2500 CEMENT CONCRETE.
2. DRIVEWAY APRON SHALL BE POURED MONOLITHICALLY WITH CURB AND GUTTER. 6" OF C.M.B SHALL BE PLACED UNDER THE DRIVEWAY, CURB AND GUTTER.
3. ADJACENT SIDEWALKS 5' ON EITHER END OF DRIVEWAY SHALL BE POURED 8" THICK
4. 10" DIA. PIPE BARRIER MIN. SCHEDULE 30 PER CITY STD. V2122 SHALL BE INSTALLED AS NEEDED TO PROTECT IMPROVEMENTS.
5. ALL CONCRETE SHALL BE ONE COURSE, STEEL TROWEL & MEDIUM HAIR BROOM FINISH. NO TOPPING PERMITTED.
6. DRIVEWAYS MAY BE OPENED TO TRAFFIC:
  - A. IN 48 HOURS WITH HIGH STRENGTH CEMENT
  - B. IN 5 DAYS WITH STANDARD CLASS 520-C-2500 MIX.
7. ENDS OF DRIVEWAYS TO BE NOT LESS THAN 5' FROM NEAREST FIRE HYDRANT, METER BOX OR VALVE AND 12' FROM UTILITY POLE.
8. 1/4" EXPANSION JOINT FELT SHALL BE PLACED ALONG THE TX.
9. DRIVEWAY ENTRANCE WIDTH (W) SHALL BE:
  - A. 25' MIN FOR NON-TRUCK USE.
  - B. 40' MIN FOR TRUCK USE.
10. 30" CONCRETE OR MASONRY WALL PER STD. V2122 OR VARIABLE HEIGHT CURB IF WROUGHT IRON FENCE OR LANDSCAPING IS INSTALLED.

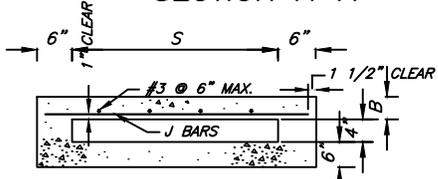
STANDARD DRIVEWAY

CITY OF VERNON, CALIFORNIA  
PUBLICWORKS, WATER & DEVELOPMENT SERVICES  
FEBRUARY 2016

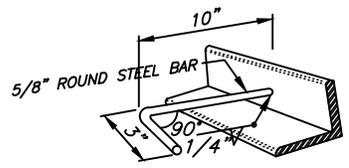
STANDARD PLAN  
PV 693  
PAGE 2 OF 2



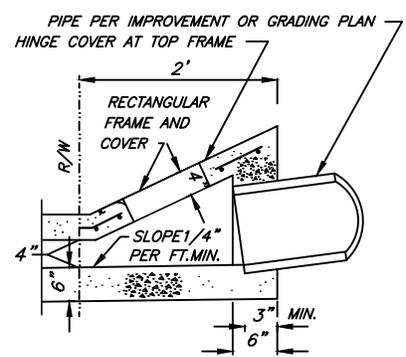
**SECTION A-A**



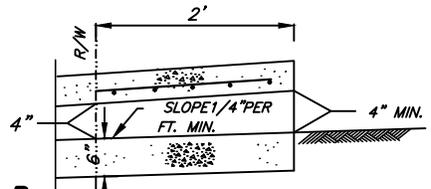
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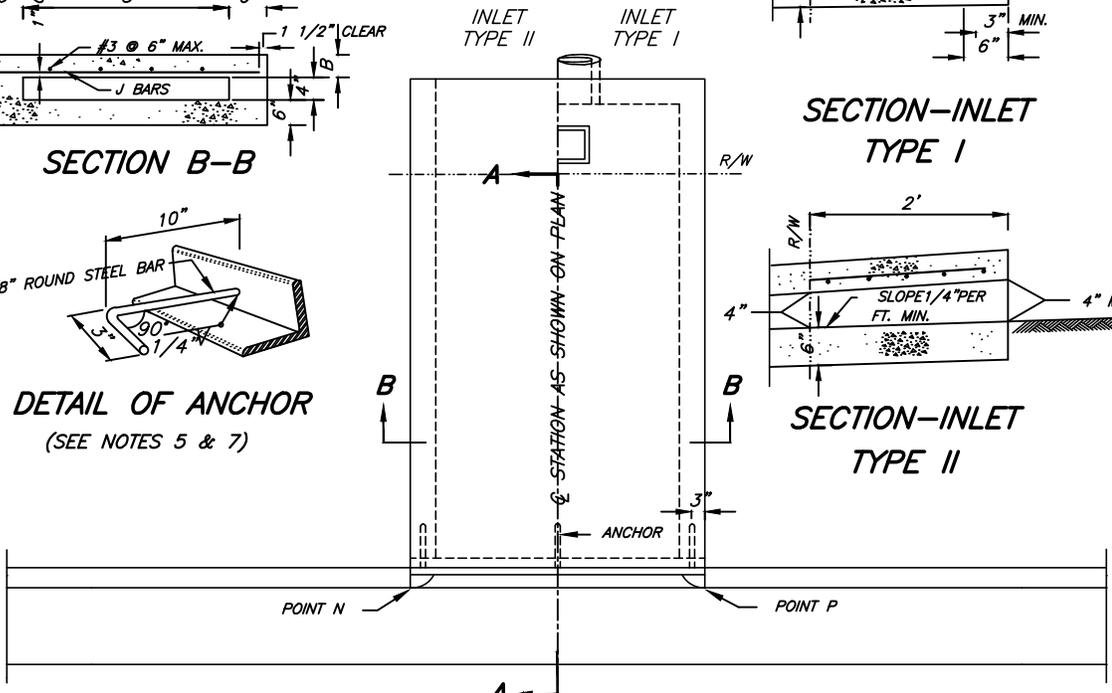
**DETAIL OF ANCHOR**  
(SEE NOTES 5 & 7)



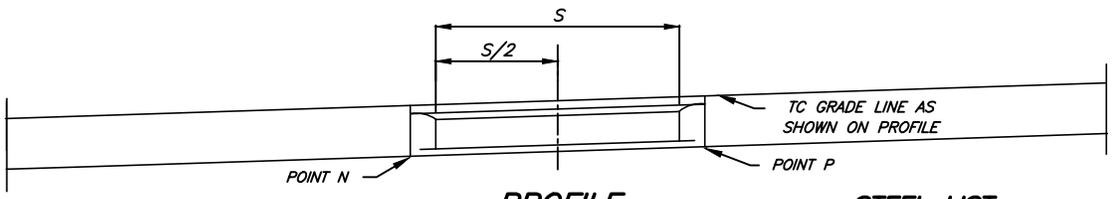
**SECTION-INLET TYPE I**



**SECTION-INLET TYPE II**



**PLAN**



**PROFILE**

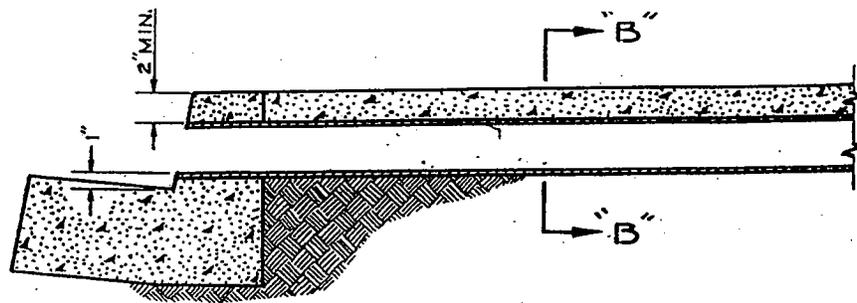
**STEEL LIST**

S	B	GALVANIZED STEEL ANGLE	ANCHOR	J BAR		
				SIZE	SPACING	LENGTH
1'-0"	3"	2 1/2" X 2" X 3/8"	2	#3	7"	1'-9"
1'-6"	"	"	"	"	"	2'-3"
2'-0"	"	"	"	"	"	2'-9"
2'-6"	"	"	"	"	"	3'-3"
3'-0"	"	"	3	"	"	3'-9"
3'-6"	"	"	"	"	6"	4'-3"
4'-0"	"	"	"	"	5"	4'-9"

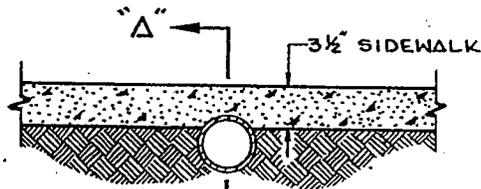
**NOTES:**

- FLOOR OF BOX TO BE TROWELED SMOOTH.
- WHEN THE TOE OF SLOPE IS WITHIN THE R/W INLET TYPE I BEGINS AT THE TOE RATHER THAN AT THE R/W LINE.
- FOR OPEN DITCH APPROACH (TYPE II) THE 2' EXTENSION IS NOT REQUIRED WHEN THE BACK OF WALK IS 2' OR MORE FROM THE R/W LINE.
- TOP OF INLET STRUCTURE (TYPE I & II) TO BE FLUSH WITH ADJACENT SURFACE WHERE PRACTICABLE.
- A HEADED STEEL STUD 5/8" X 6 3/8" WITH HEAD D=1" ATTACHED BY A FULL PENETRATION BUTT WELD MAY BE USED AS AN ALTERNATE ANCHOR.
- THE 3" LEG OF THE INTERIOR ANCHORS SHALL BE PARALLEL TO THE TOP OF SIDEWALK.

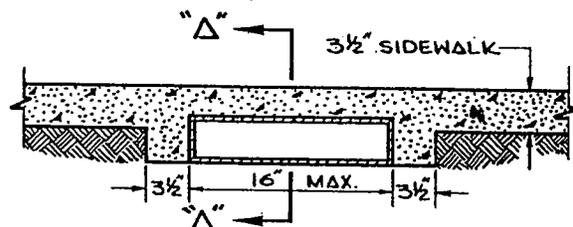
**PARKWAY DRAIN NO.1**  
CITY OF VERNON, CALIFORNIA  
PUBLIC WORKS DEPARTMENT  
MAY, 1991  
STANDARD PLAN  
**DV 2171**



SECTION "Δ"-Δ  
TYPICAL - TYPE 1 & 2



SECTION "B"-B  
TYPE 1



SECTION "B"-B  
TYPE 2

NOTES:

1. MATERIALS FOR TYPE 1 DRAINS SHALL BE STEEL, CAST IRON OR P.V.C. MATERIALS FOR TYPE 2 DRAINS SHALL BE STEEL OR CAST IRON ONLY.
2. REFERENCE IS MADE TO STANDARD PLAN PV 582 - STANDARD SIDEWALK, CURB AND GUTTER.

**CURB DRAINS**

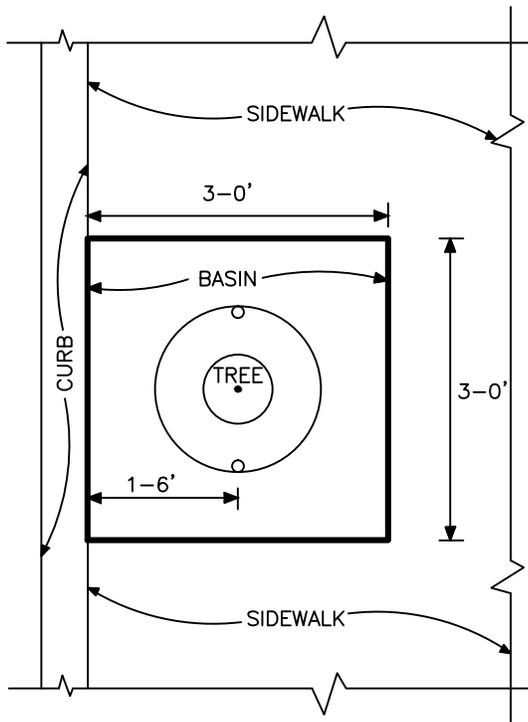
CITY OF VERNON, CALIFORNIA

PUBLIC WORKS DEPARTMENT

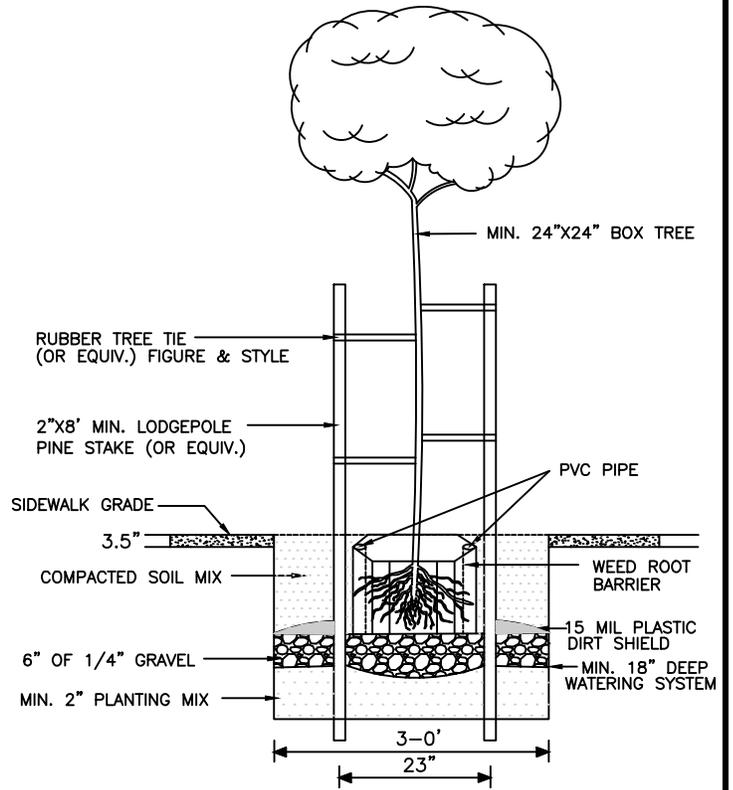
AUGUST, 1971

STANDARD PLAN

DV 1888



PLAN VIEW



SIDE VIEW

NOTES:

1. TREE SHALL BE IN 24" X 24" BOX AND HAVE MINIMUM DIMENSIONS OF 8 FT HIGH, 3 FT WIDE AT THE HEAD AND A TRUNK OF 2 INCH.
2. TREE SPACING 100' OR PER CITY ENGINEER'S DIRECTION.
3. PLANT 25' MIN. FROM B.C.R. AT INTERSECTIONS.
4. PLANT 25' MIN. FROM EDGE OF DRIVE APPROACH.
5. PLANT 10' MIN. FROM UTILITY AND SEWER LINES.
6. PLANT 21' MIN. FROM STREET LIGHT STANDARDS.
7. PLANT 15' MIN. FROM POWER POLES.
8. PLANT 10' MIN. FROM FIRE HYDRANTS.
9. ONE YEAR WARRANTY FOR LOSS OR DAMAGE REQUIRED.

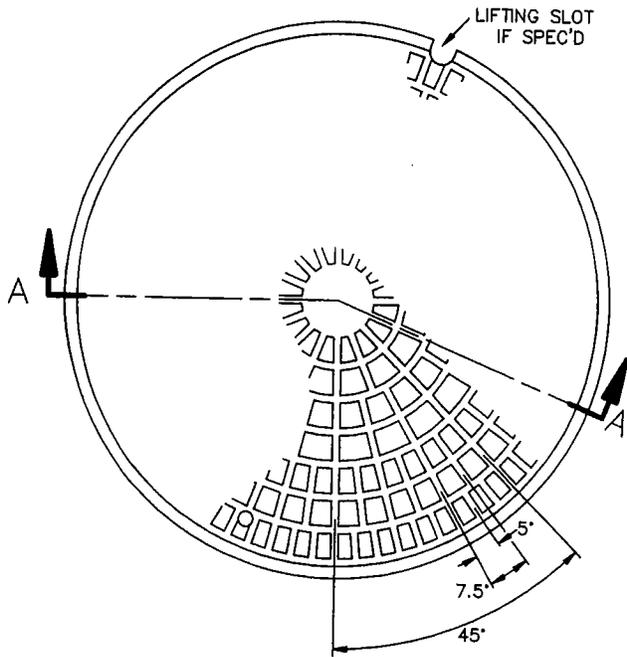
# STANDARD TREE PLANTING

CITY OF VERNON, CALIFORNIA  
COMMUNITY SERVICES & WATER DEPARTMENT  
MARCH 2006

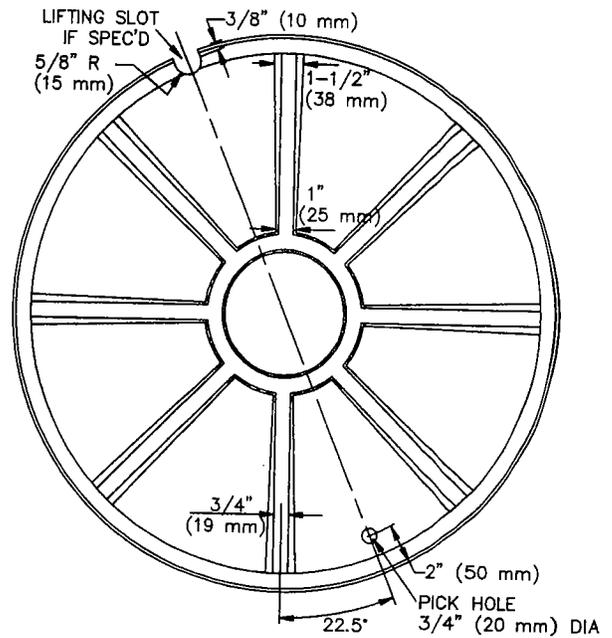
STANDARD PLAN

**MV 1891**

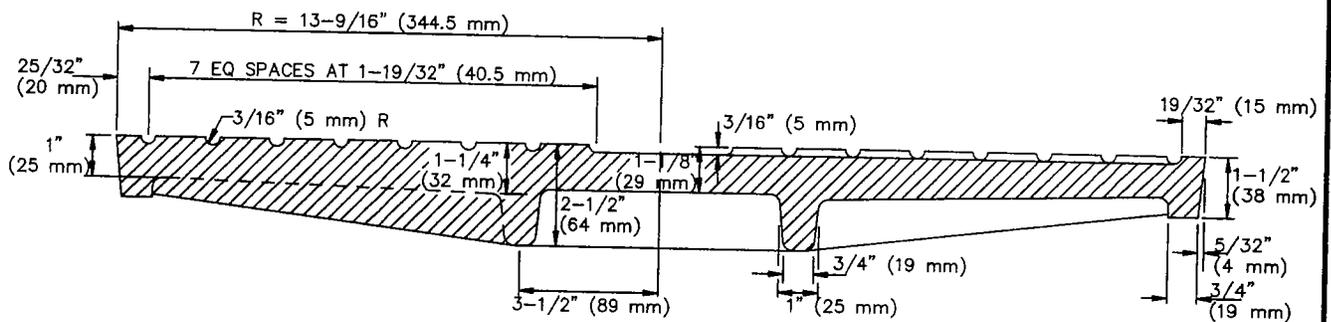
REVISIONS		
DATE	DESCRIPTION	INITIALS
10/6/03	3'X3' TREE WELL/UPDATED	LM
3/14/06	24" X 24" BOX TREE AND TRUNK 2" MIN.	PC



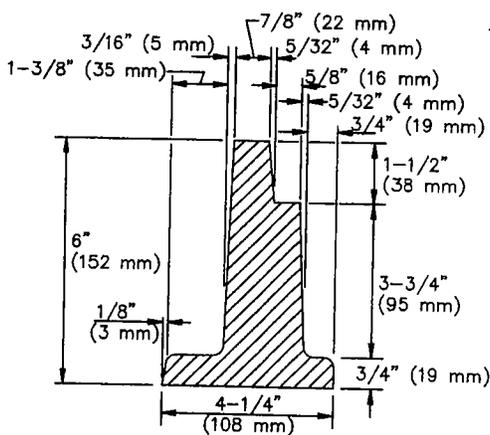
TOP OF COVER



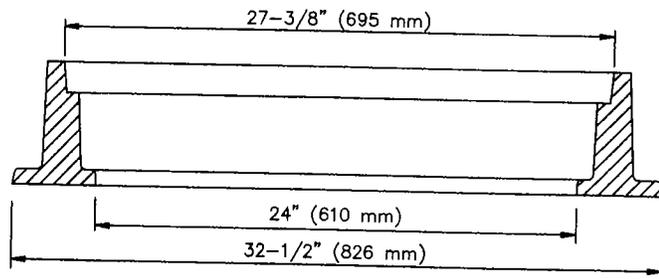
BOTTOM OF COVER



SECTION A-A



FRAME DETAIL



SECTION THRU FRAME

STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION

PROMULGATED BY THE  
PUBLIC WORKS STANDARDS INC.  
GREENBOOK COMMITTEE  
1984  
REV. 1990, 2009

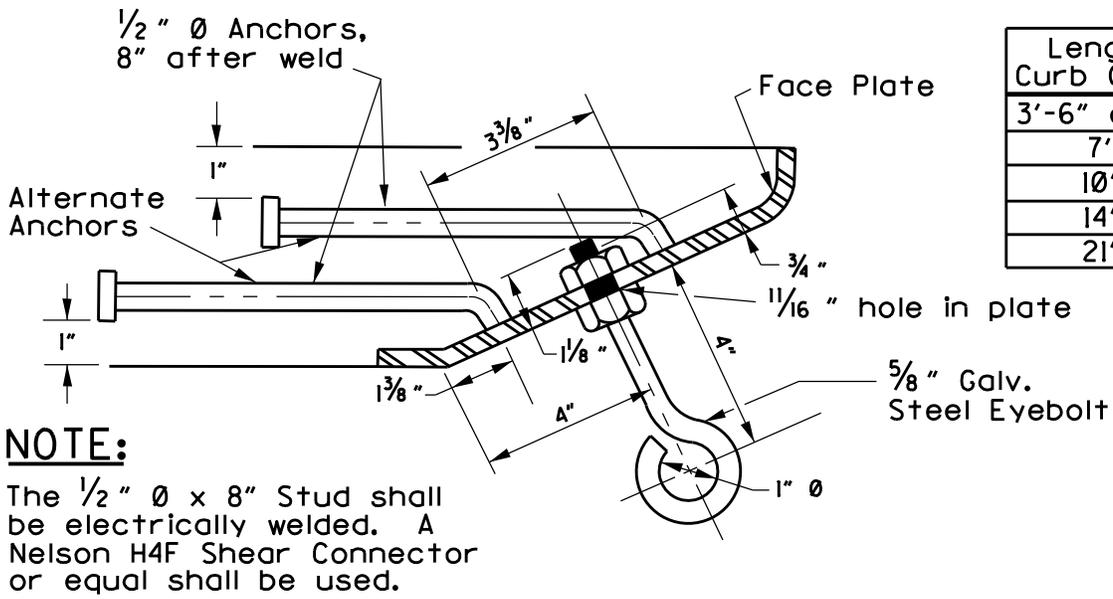
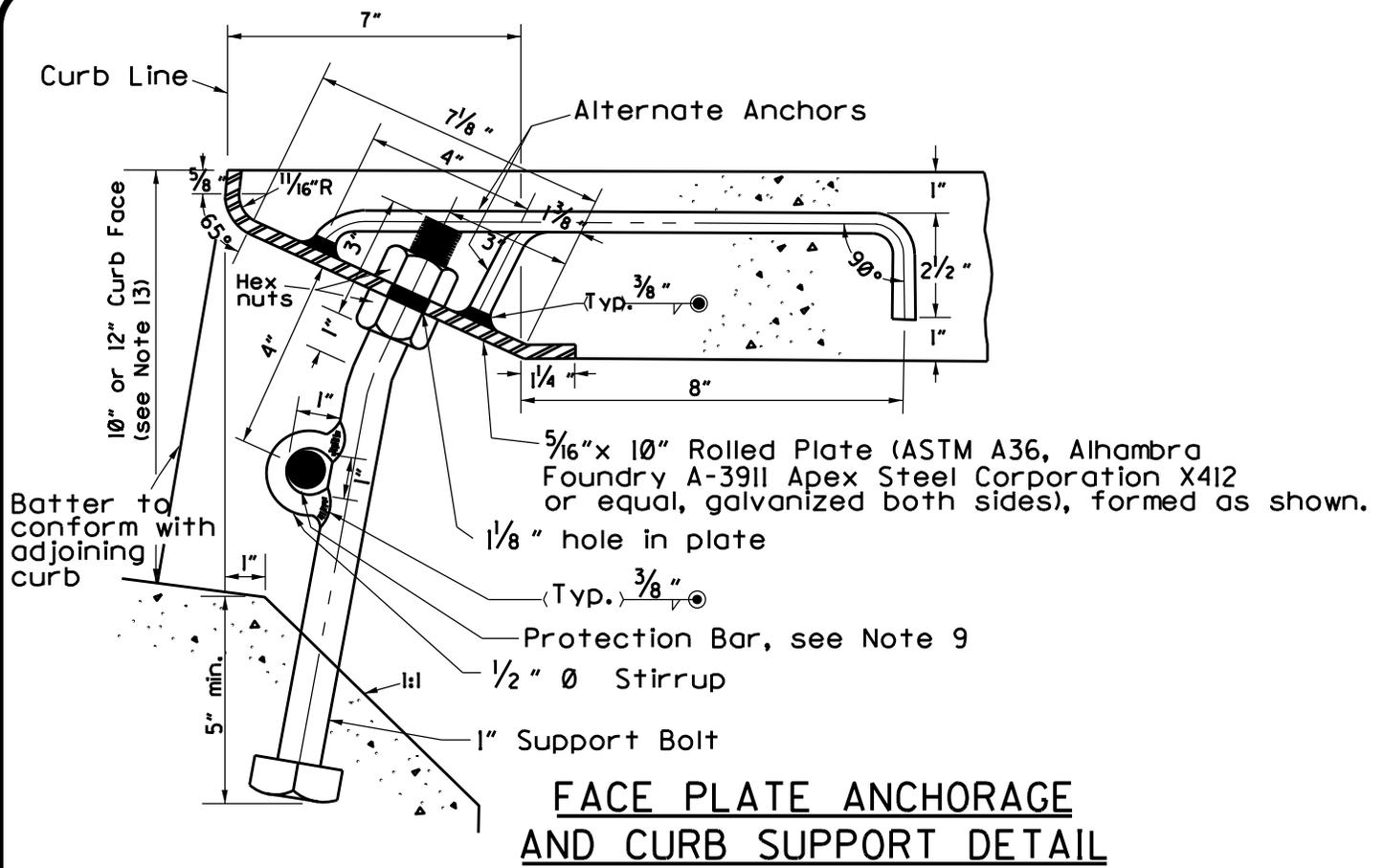
**24" (610 mm) MANHOLE FRAME  
AND COVER**

STANDARD PLAN

**630-3**

USE WITH STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION

SHEET 1 OF 2



Length of Curb Opening	No. of Anchors
3'-6" or less	2
7'-0"	3
10'-0"	4
14'-0"	5
21'-0"	7

**NOTE:**  
 The 1/2" Ø x 8" Stud shall be electrically welded. A Nelson H4F Shear Connector or equal shall be used.

COUNTY OF ORANGE, OC PUBLIC WORKS DEPARTMENT

Approved   
 Ignacio G. Ochoa, Chief Engineer

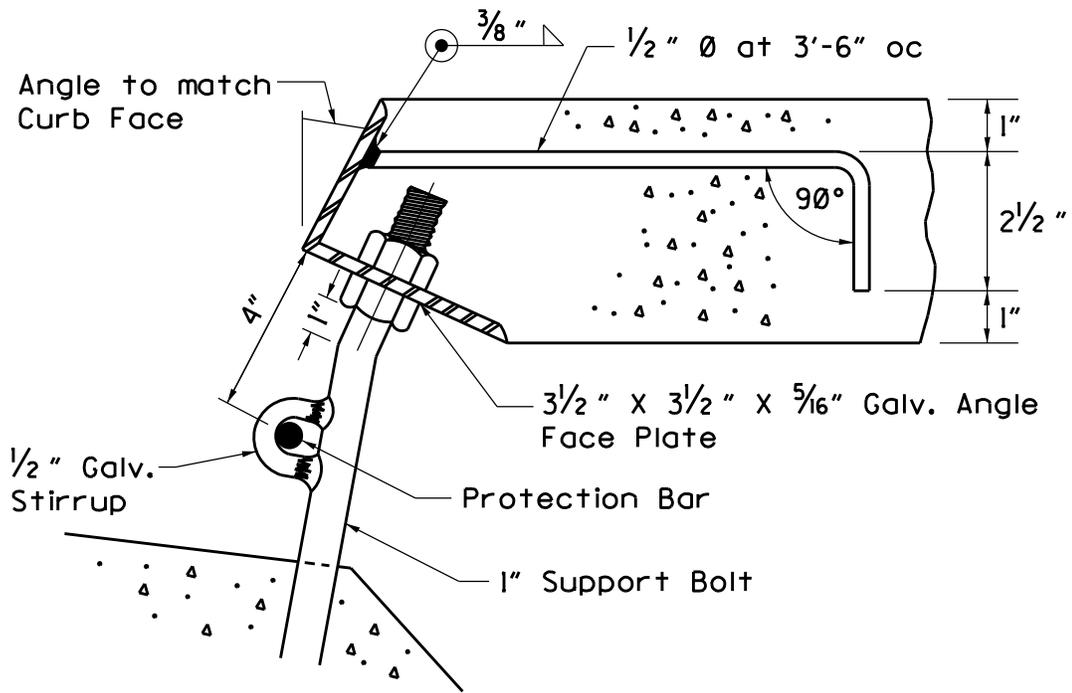
Revision: April 2013

STD. PLAN

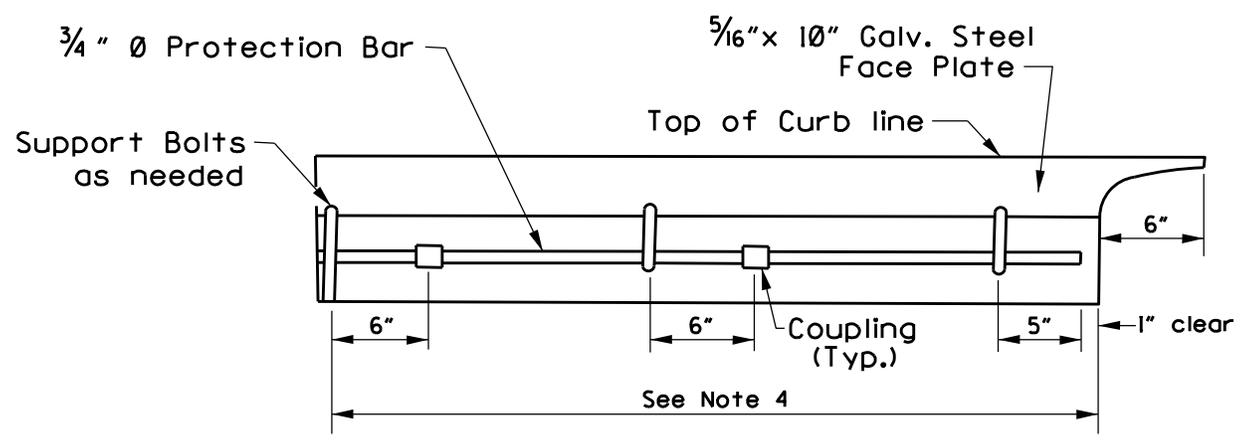
1306

MISCELLANEOUS CURB INLET DETAILS AND NOTES

SHT. 1 OF 3



**DETAIL A**



**PROTECTION BAR DETAIL**

COUNTY OF ORANGE, OC PUBLIC WORKS DEPARTMENT

Approved   
 Ignacio G. Ochoa, Chief Engineer

Revision: April 2013

STD. PLAN

**1306**

**MISCELLANEOUS CURB INLET DETAILS AND NOTES**

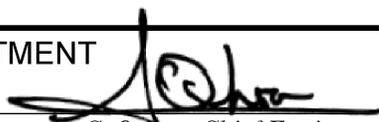
SHT. 2 OF 3

## NOTES:

1. One eyebolt shall be placed 12 inches from each end of face plate.
2. Eyebolts shall be symmetrically spaced in the curb opening so that the unsupported span is not more than 4 feet; support bolts, when used, should be considered as eyebolts in the spacing.
3. One coupling shall be placed 6 inches to the right or left of each eyebolt with the exception of the last eyebolt. Couplings shall be threaded to facilitate removal of protection bar.
4. Galvanized steel support bolts shall be installed when length of curb opening exceeds 7-feet and shall be spaced at not more than 7 feet on-center and not less than 5 feet on-center.
5. Face plate anchors shall be uniformly spaced not to exceed 4 feet between centers and shall be placed  $4\frac{1}{2}$  inches from each end of the face plate. An anchor shall be placed over each W-beam in a grating inlet.
6. A coupling may be omitted provided the protection bar is removable after installation.
7. All metal shall be galvanized after fabrication.
8. Support bolts, eyebolts, and anchors may be attached by a full penetration butt weld as an alternate.
9. Place a  $\frac{3}{4}$ -inch dia. protection bar horizontally across the entire length of the curb opening.
10. See Std. Plan 1307 for additional miscellaneous inlet details and notes.
11. Curb section shall match adjacent curb type.
12. Curb opening shall conform to curb alignment.
13. Height of curb opening will vary with the type of curb and the depth of the local depression (Std. Plan 1308).
14. The angle as shown on detail A shall be allowed only when on approved plans.

COUNTY OF ORANGE, OC PUBLIC WORKS DEPARTMENT

Approved

  
Ignacio G. Ochoa, Chief Engineer

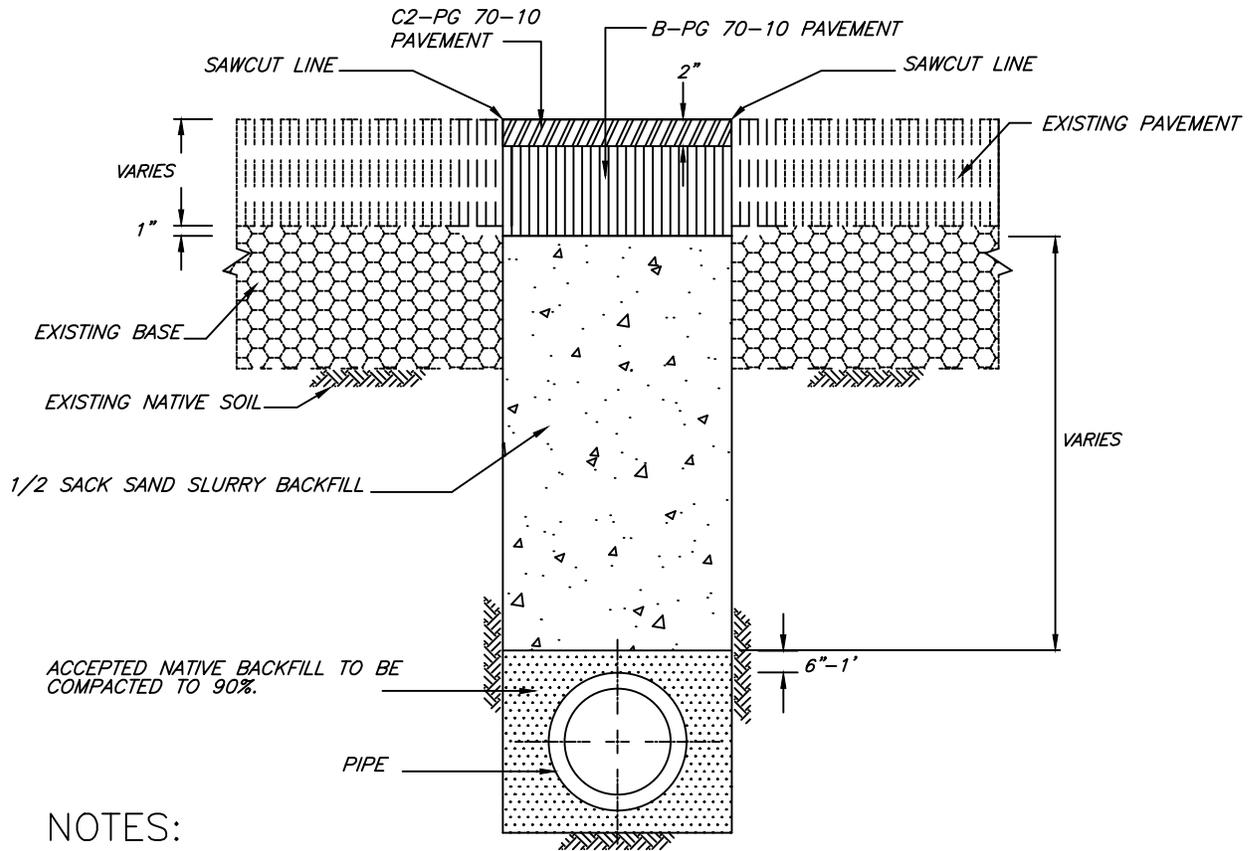
Revision: April 2013

STD. PLAN

1306

MISCELLANEOUS CURB INLET DETAILS AND NOTES

SHT. 3 OF 3



**NOTES:**

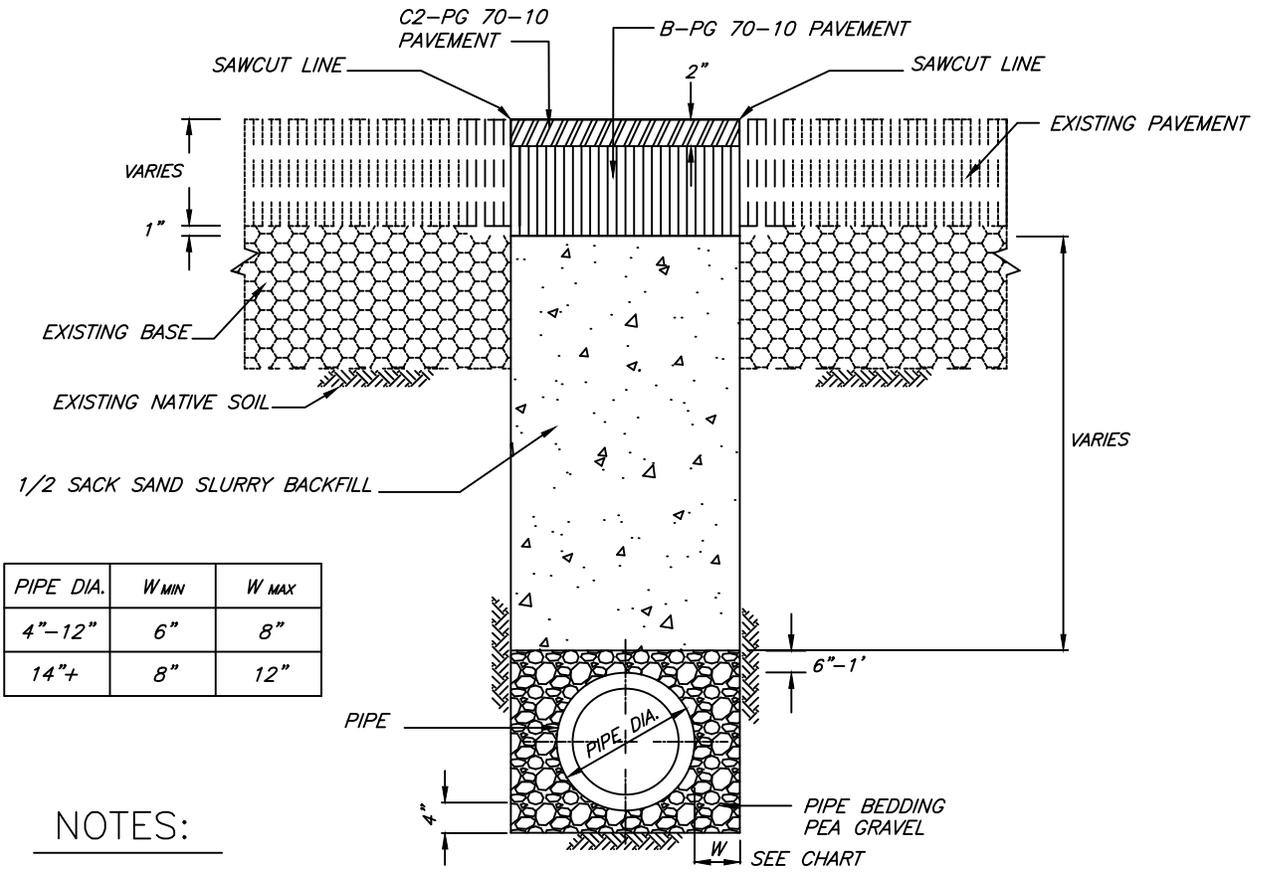
1. NEW PAVEMENT THICKNESS SHALL BE 1 INCH BELOW THE EXISTING TO A MAXIMUM OF 10 INCHES, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
2. ASPHALT CONCRETE SHALL BE B-PG70-10. TACK COAT SHALL BE APPLIED PER LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (GREENBOOK).
3. FINISHED ASPHALT CONCRETE SHALL BE C2-PG70-10, 2" THICK AND PAVED FLUSH WITH ADJACENT PAVEMENT WITHIN SAME DAY AS NEW A.C. PLACEMENT.
4. DAMAGED AND UNDERMINED PAVEMENT SHALL BE REMOVED BY SAW CUTTING FULL DEPTH PARALLEL TO TRENCH AND REPLACED WITH ASPHALT CONCRETE PER ABOVE AT NO COST TO THE CITY.
5. FOR CONCRETE STREETS THE WIDTH OF CONCRETE TO BE REPLACED SHALL EITHER BE FROM COLD JOINT LINE TO COLD JOINT LINE OR APPROVED TRENCH WIDTH WITH CONCRETE DOWELS (MIN. #4 BARS) PLACED EVERY 3 FT STAGGERED.
6. IF THE SAWCUT LINE IS 3 FEET OR LESS FROM GUTTER LINE OR COLD JOINT THE A.C. PAVEMENT OR CONCRETE SHALL BE REMOVED TO GUTTER LINE OR COLD JOINT.
7. ALL TRAFFIC SIGNAL LOOPS, DOTS, LANE LINES, PEDESTRIAN LINES, AND OTHER PAINTED MARKINGS ARE TO BE REPLACED IN KIND BY THE CONTRACTOR.
8. AN ENCROACHMENT PERMIT SHALL BE OBTAINED FROM THE CITY OF VERNON COMMUNITY SERVICES AND WATER DEPARTMENT PRIOR TO ANY ENCROACHMENT OR CONSTRUCTION WITHIN A CITY OF VERNON RIGHT-OF-WAY.
9. THE CONTRACTOR SHALL OBTAIN AN UNDERGROUND SERVICE ALERT TICKET AND OBTAIN A CALIFORNIA COMMISSION OF OCCUPATIONAL SAFETY AND HEALTH (DOSH) PERMIT IF THE TRENCH IS GREATER THAN 5 FEET DEEP PRIOR TO THE COMMENCEMENT OF WORK.
10. THE CONTRACTOR IS RESPONSIBLE FOR FURNISHING AND MAINTAINING ALL TRAFFIC CONTROLS AND SIGNAGE PER THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) OR THE CALIFORNIA JOINT UTILITY TRAFFIC CONTROL MANUAL (CJUTCM) DURING ENTIRE PROJECT.
11. NO STOCK PILING OF CONSTRUCTION MATERIALS OR EQUIPMENT SHALL BE ALLOWED OVER NIGHT IN THE PUBLIC RIGHT-OF-WAY UNLESS APPROVED BY THE CITY ENGINEER.
12. ALL NECESSARY STEEL PLATES SHALL BE PROVIDED AT THE JOB SITE PRIOR TO ANY REMOVALS. PLATES SHALL BE SKID RESISTANT, RESTRAINED AND RAMPED WITH A.C. COLD MIX.
13. ALL TRENCHES SHALL BE BACKFILLED WITH 1/2 SACK SAND SLURRY UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
14. THE CONSTRUCTION SHALL COMPLY WITH CITY STANDARDS AND THE GREEN BOOK.

**TYPICAL TRENCH PAVING SECTION**

REVISIONS		
DATE	DESCRIPTION	INITIALS
11/20/02	LAST UPDATE	L.M.
12/2/02	ADDED NOTE 12.	L.M.
11/12/03	UPDATED NOTE 3.	L.M.
5/25/04	ADDED NOTE 3.	L.M.
2/14/05	UPDATED NOTE 2, 3 AND 4.	P.C.
10/18/06	UPDATED NOTES 3 & 4.	L.M.
11/21/06	UPDATED NOTES 2, 3 & 4.	L.M.

CITY OF VERNON, CALIFORNIA  
 COMMUNITY SERVICES & WATER DEPARTMENT  
 OCTOBER 2006

STANDARD PLAN V2264  
 SHEET 1 OF 2



**NOTES:**

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14. THE CONSTRUCTION SHALL COMPLY WITH CITY STANDARDS AND THE GREEN BOOK.

**TYPICAL TRENCH PAVING SECTION FOR SEWER LATERAL**

STANDARD PLAN V2264  
SHEET 2 OF 2

REVISIONS		
DATE	DESCRIPTION	INITIALS
11/20/02	LAST UPDATE	L.M.
12/2/02	ADDED NOTE 12.	L.M.
11/12/03	UPDATED NOTE 3.	L.M.
8/28/04	ADDED NOTE 3.	L.M.
2/14/05	UPDATED NOTE 2, 3 AND 4.	P.C.
10/18/06	UPDATED NOTES 3 & 4.	L.M.
11/21/06	UPDATED NOTES 2, 3 & 4.	L.M.

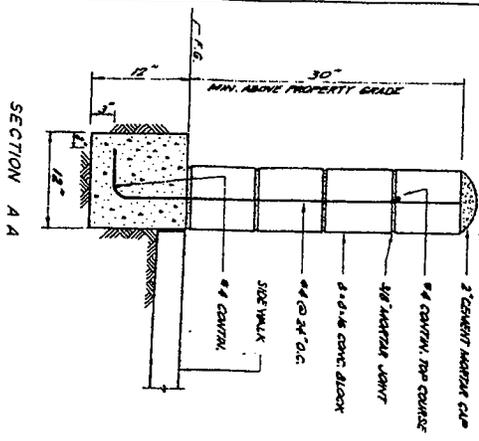
CITY OF VERNON, CALIFORNIA  
COMMUNITY SERVICES & WATER DEPARTMENT  
OCTOBER 2006

DATE	REVISION	NAME
0-0-53	ISSUED	F.A.

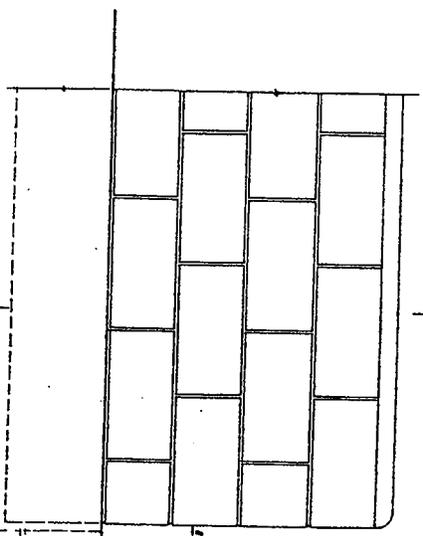
STANDARD MASONRY WALL BARRIER

CITY OF VERNON  
BUILDING DEPARTMENT

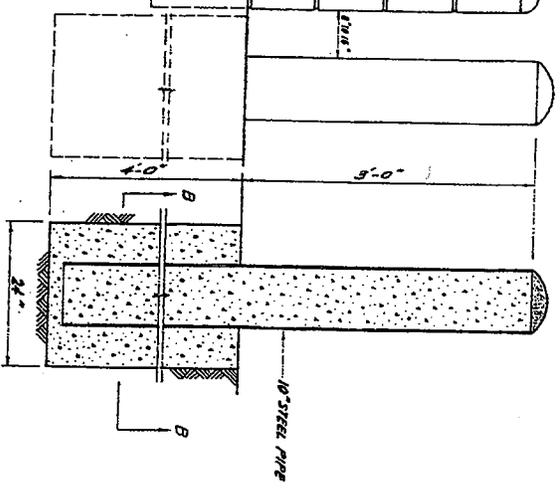
JUNE 1988



SECTION A A

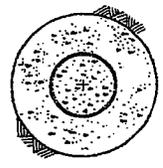


ELEVATION



SECTION B B

TYPICAL GUARD POST



NOTES:

1. BARRIERS ARE REQUIRED PER CITY OF VERNON ZONING ORDINANCE SECTION 26.4.2-8 (9) WHERE PARKING AREAS ADJACENT PUBLIC STREETS.
2. INSPECTIONS ARE REQUIRED PRIOR TO POURING FOOTINGS AND GROUTING.
3. WALL FOOTINGS AND GUARD POST FOOTINGS SHALL NOT PROTRUDE INTO PUBLIC RIGHT-OF-WAY.
4. FILL ALL BLOCK CELLS WITH GROUT.
5. GUARD POSTS AND WALLS SHALL HAVE INDEPENDENT FOOTINGS.
6. WEEP HOLES ARE REQUIRED WHERE PARKING AREAS DRAIN OVER THE SIDEWALKS.
7. PROVIDE 1/4" INCH EXPANSION JOINTS IN WALLS AT 4'-0" O.C. MAX.
8. GROUTING OF ANY SECTION OF WALL SHALL BE COMPLETED WITH NO INTERRUPTIONS GREATER THAN ONE HOUR.
9. LAPPING OF REINFORCING BARS SHALL NOT BE LESS THAN 2'-4" FOR HORIZONTAL BARS AND NOT LESS THAN 1'-2" FOR VERTICAL BARS.
10. ANY ALTERNATIVE OR DEVIATION FROM THIS STANDARD PLAN REQUIRES APPROVAL FROM THE DIRECTOR OF COMMUNITY SERVICES.
11. STANDARD GUARD POST SHALL BE CONSTRUCTED AT ENDS OF WALL BARRIER AT DRIVEWAYS.
12. 10" DIA. PIPE BARRIER MIN. SCHEDULE 30 SHALL BE INSTALLED AS SHOWN.
13. AFTER PLACEMENT OF REBAR, APPROVED SAFETY CAPS SHALL BE PLACED ON ENDS OF ALL EXPOSED REBAR.